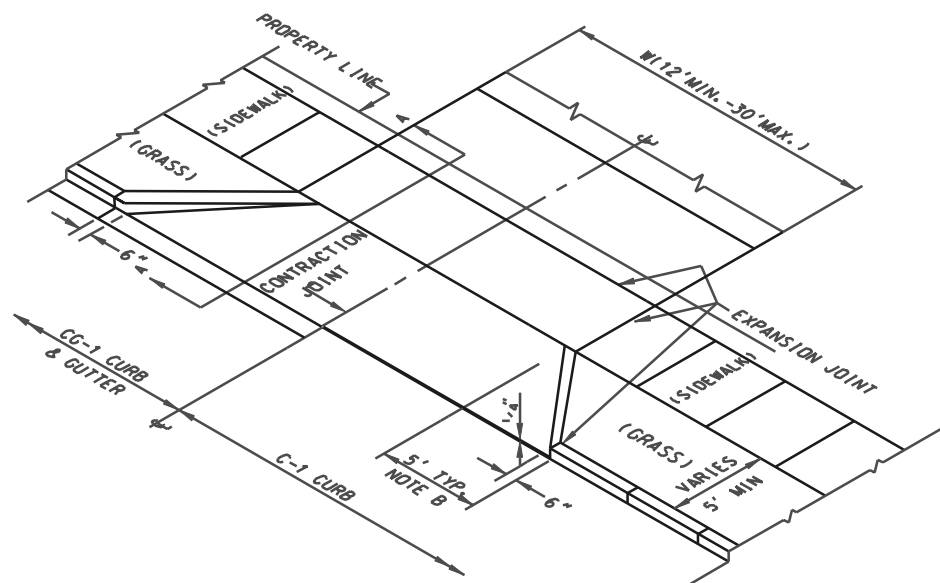
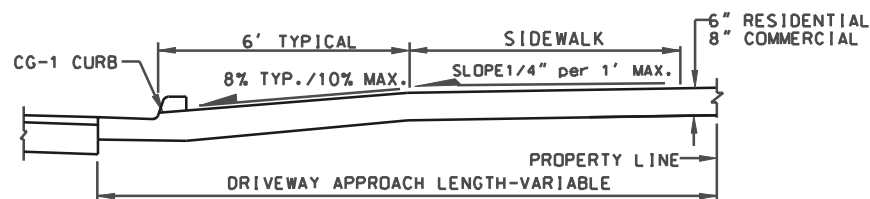


DRIVEWAY ENTRANCES

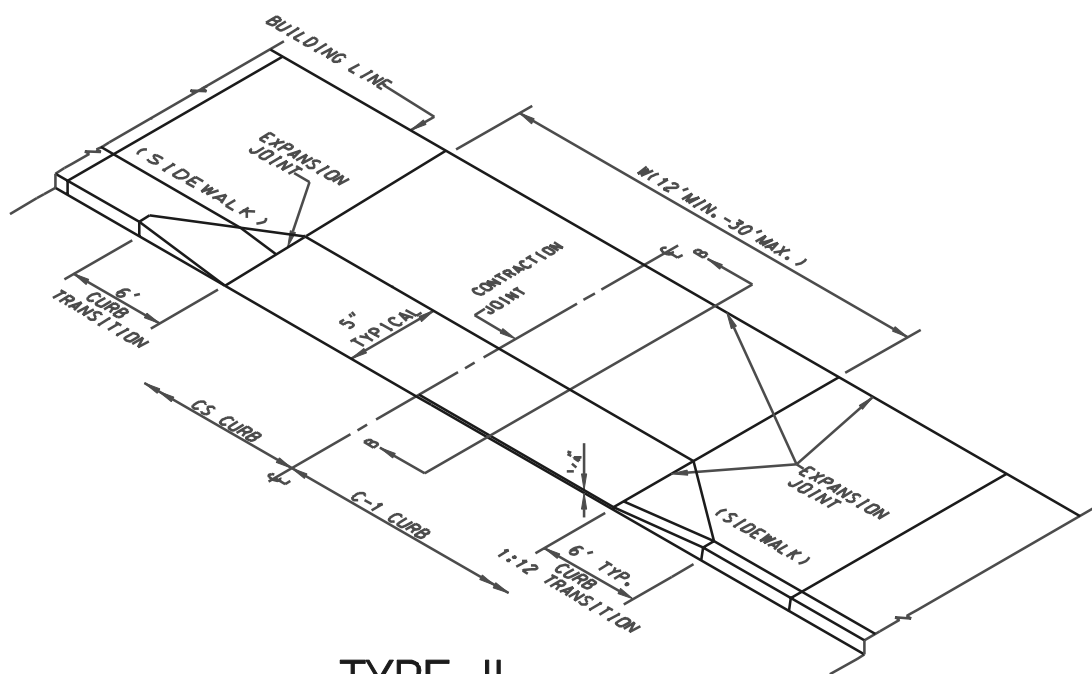


TYPE I

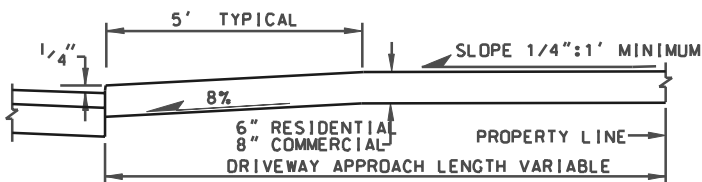


SECTION A-A (CG-1 CURB SHOWN)

- A. TYPE I LAYOUT MAY BE ADAPTED TO CG-2 CURB.
- B. WHERE DRIVE WIDTH IS 14 FEET OR LESS FLARES SHALL BE 7.5 FEET WIDE AT CURB AND SHALL EXTEND AT A 45° ANGLE THROUGH THE SIDEWALK.



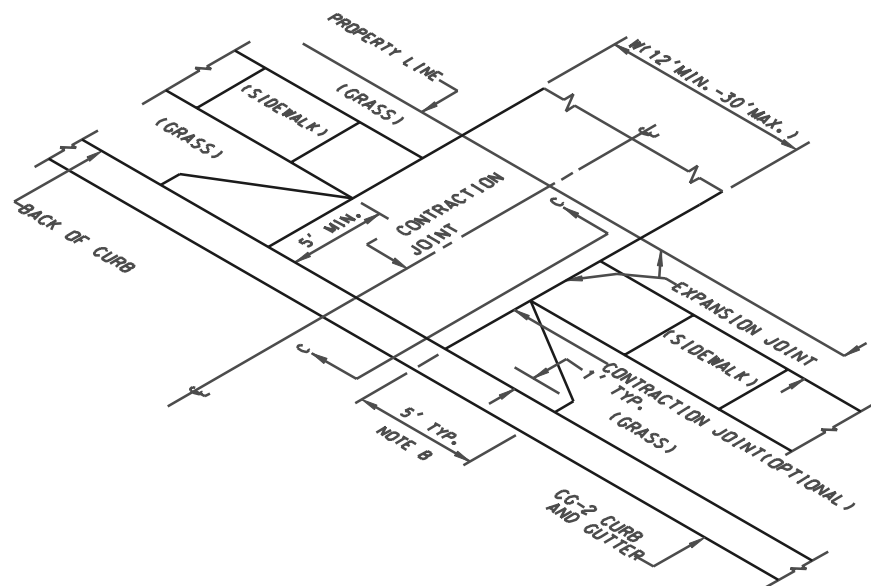
TYPE II



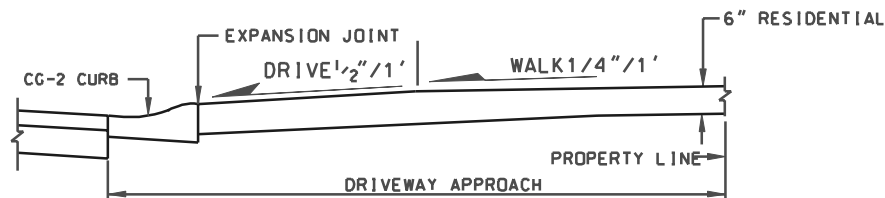
SECTION B-B (CS OR C-1 CURB)

NOTES:

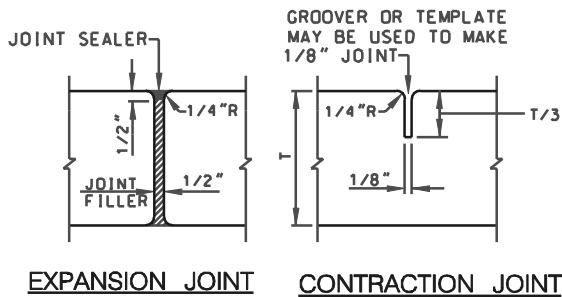
1. SIDEWALK CROSS SLOPES THROUGH DRIVEWAY MUST BE 1/4" PER FT. TO COMPLY WITH THE AMERICAN DISABILITY ACT.
2. THE TOP 6" OF DRIVEWAY SUBGRADE SHALL BE COMPACTED TO 95% OF STANDARD MAXIMUM DENSITY.
3. THAT PORTION OF THE DRIVE WITHIN STREET R.O.W. SHALL BE PCC CONCRETE. CONCRETE SHALL CONFORM TO MCIB MIX NO. WA610-1-4-0.41, EXCEPT IN CBD WHERE WA622-1/2-2-0.41 WITH TRAP ROCK AGGREGATE IS REQUIRED.
4. EXPANSION JOINT FILLER AND JOINT SEALING COMPOUND SHALL CONFORM TO STANDARD SPECIFICATIONS SECTION 2209.2.
5. CURING MEMBRANES SHALL CONFORM TO STANDARD SPECIFICATIONS SECTION 2301.5
6. CURB TRANSITIONS ON DRIVEWAY FLARES ARE CONSIDERED PART OF DRIVEWAY.
7. IN CBD, 6 x 6-W2.9 x W2.9 REINFORCING SHALL BE PLACED IN CENTER OF SLAB THICKNESS.
8. ON PARK DEPT. PROPERTY PLACE 6 x 6-W1.4 x W1.4 REINFORCING IN CENTER OF SLAB THICKNESS AND USE RADIUS INSTEAD OF FLARE.
9. CONTRACTION JOINTS SHALL BE SPACED AT 12' MAX. BOTH DIRECTIONS.
10. FORM 1/4" LIP AT PAVEMENT LINE ON DRIVES IN C-1 & CS CURBS.
11. MINIMUM SIDEWALK WIDTHS:
4' RESIDENTIAL, 5' HIGHER CLASS STREET



TYPE III



SECTION C-C (CG-2 CURB)



JOINT DETAILS

ADOPTED

Director of Public Works Entry No. Date

Kansas City, Missouri
Public Works Department
Engineering Division



DRIVEWAY
ENTRANCES

STANDARD DRAWING NUMBER
D-1